

Bridge No. 3001
BA-2718
US 1 over Little Gunpowder Falls
Reckord vicinity
1934
Public

Bridge No. 3001, constructed circa 1934, is a single-span, metal girder bridge that carries US 1 over Little Gunpowder Falls. It is 68 feet long and 48 feet wide. The bridge's solid concrete parapets replace the original open balustrade. Steel w-beam guardrails are attached to incised endposts. The substructure consists of horizontally-incised concrete abutments and flared wing walls.

The Maryland State Roads Commission (SRC) built Bridge No. 3001 to serve increased traffic density, resulting from the assignment of this road as US 1. The bridge is a significant example of a metal girder bridge, retaining concrete encased rolled steel girders and concrete abutments and wing walls, characteristic of the SRC's work in the 1930s. Included in Maryland State Highway Administration's 1996 Historic Bridge Inventory, the bridge was determined eligible for the National Register of Historic Places in 2001 under Criterion C. However, its deteriorated condition necessitates its demolition.

Maryland Historical Trust Maryland Inventory of Historic Properties Form

Inventory No. BA-2718

1. Name of Property (indicate preferred name)

historic Bridge No. 3001
 other US 1 over Little Gunpowder Falls

2. Location

street and number US 1 (Belair Road) over Little Gunpowder Falls not for publication
 city, town Reckord vicinity
 county Baltimore

3. Owner of Property (give names and mailing addresses of all owners)

name Maryland State Highway Administration
 street and number 707 N. Calvert Street telephone 800-548-5026
 city, town Baltimore state MD zip code 21202

4. Location of Legal Description

courthouse, registry of deeds, etc. N/A liber N/A folio N/A
 city, town N/A tax map N/A tax parcel N/A tax ID number N/A

5. Primary Location of Additional Data

- Contributing Resource in National Register District
- Contributing Resource in Local Historic District
- Determined Eligible for the National Register/Maryland Register
- Determined Ineligible for the National Register/Maryland Register
- Recorded by HABS/HAER
- Historic Structure Report or Research Report at MHT
- Other: Historic Bridge Inventory

6. Classification

Category	Ownership	Current Function	Resource Count	
<input type="checkbox"/> district	<input checked="" type="checkbox"/> public	<input type="checkbox"/> agriculture	Contributing	Noncontributing
<input type="checkbox"/> building(s)	<input type="checkbox"/> private	<input type="checkbox"/> commerce/trade	<u>0</u>	<u>0</u> buildings
<input checked="" type="checkbox"/> structure	<input type="checkbox"/> both	<input type="checkbox"/> defense	<u>0</u>	<u>0</u> sites
<input type="checkbox"/> site		<input type="checkbox"/> domestic	<u>1</u>	<u>0</u> structures
<input type="checkbox"/> object		<input type="checkbox"/> education	<u>0</u>	<u>0</u> objects
		<input type="checkbox"/> funerary	<u>1</u>	<u>0</u> Total
		<input type="checkbox"/> government		
		<input type="checkbox"/> health care		
		<input type="checkbox"/> industry		
		<input type="checkbox"/> landscape		
		<input type="checkbox"/> recreation/culture		
		<input type="checkbox"/> religion		
		<input type="checkbox"/> social		
		<input checked="" type="checkbox"/> transportation		
		<input type="checkbox"/> work in progress		
		<input type="checkbox"/> unknown		
		<input type="checkbox"/> vacant/not in use		
		<input type="checkbox"/> other:		
			Number of Contributing Resources previously listed in the Inventory <u>1</u>	

7. Description

Inventory No. BA-2718

Condition

excellent deteriorated
 good ruins
 fair altered

Prepare both a one paragraph summary and a comprehensive description of the resource and its various elements as it exists today.

Bridge No. 3001, constructed circa 1934, is a single-span, metal girder bridge that carries US 1 over Little Gunpowder Falls. The bridge is four lanes wide and 68 feet in length. Its notable characteristics include poured concrete parapets and horizontally-incised concrete abutments and wing walls.

The bridge carries US 1 (Belair Road) north and south over Little Gunpowder Falls in Baltimore County, Maryland, just south of the Harford County boundary. The four-lane approach of US 1 is level but slightly curved north of the bridge. The bridge is located within Gunpowder Falls State Park, and the area immediately surrounding the bridge is characterized as wooded and rural. Little Gunpowder Falls flows from west to east; Wildcat Branch enters the Little Gunpowder from the north about one-half mile south of the bridge.

Bridge No. 3001 is a modest, single-span structure, measuring 68 feet long. The four-lane roadway is 28 feet wide, but the deck is 46 feet wide. The deck width includes 4-foot-wide sidewalks, although there are no sidewalks on the approaching road.

Poured concrete parapets stand between capped endposts with incised panels. The parapets are panels with a smooth texture and replace an original open balustrade. The endposts are crumbling and appear to be original features; they have attached steel w-beam guardrails and show scars of the former dedication plaque. The floor system is cast-in-place concrete with preformed expansion joints. Concrete-encased, steel-rolled girders comprise the superstructure. The bridge is one span long, requiring no piers. The substructure contains horizontally-incised concrete abutments and splayed wing walls.

The bridge's current condition is significantly deteriorated. The concrete encasements, abutments, deck, sidewalks, and endposts have severe spalling that has resulted in exposed rebar, particularly in the superstructure's concrete encasements. The endposts are crumbling and there is a hole in the sidewalk. The abutments, wing walls, and concrete encasements also have efflorescence. The abutments have moderate scouring.

SHA recently constructed a 107-foot-long replacement bridge approximately 20 feet west of the existing bridge. The roadway alignment reduces the approaching curve's sharpness.

8. Significance

Inventory No. BA-2718

Period	Areas of Significance	Check and justify below		
<input type="checkbox"/> 1600-1699	<input type="checkbox"/> agriculture	<input type="checkbox"/> economics	<input type="checkbox"/> health/medicine	<input type="checkbox"/> performing arts
<input type="checkbox"/> 1700-1799	<input type="checkbox"/> archeology	<input type="checkbox"/> education	<input type="checkbox"/> industry	<input type="checkbox"/> philosophy
<input type="checkbox"/> 1800-1899	<input type="checkbox"/> architecture	<input checked="" type="checkbox"/> engineering	<input type="checkbox"/> invention	<input type="checkbox"/> politics/government
<input checked="" type="checkbox"/> 1900-1999	<input type="checkbox"/> art	<input type="checkbox"/> entertainment/ recreation	<input type="checkbox"/> landscape architecture	<input type="checkbox"/> religion
<input type="checkbox"/> 2000-	<input type="checkbox"/> commerce	<input type="checkbox"/> ethnic heritage	<input type="checkbox"/> law	<input type="checkbox"/> science
	<input type="checkbox"/> communications	<input type="checkbox"/> exploration/ settlement	<input type="checkbox"/> literature	<input type="checkbox"/> social history
	<input type="checkbox"/> community planning		<input type="checkbox"/> maritime history	<input checked="" type="checkbox"/> transportation
	<input type="checkbox"/> conservation		<input type="checkbox"/> military	<input type="checkbox"/> other: _____

Specific dates 1934 **Architect/Builder** State Roads Commission

Construction dates 1934

Evaluation for:

National Register Maryland Register not evaluated

Prepare a one-paragraph summary statement of significance addressing applicable criteria, followed by a narrative discussion of the history of the resource and its context. (For compliance projects, complete evaluation on a DOE Form – see manual.)

The Maryland State Roads Commission (SRC) built Bridge No. 3001 in 1934 as part of early twentieth-century roadway improvements. Specifically, this significant crossing that joins Baltimore and Harford counties required a bridge with greater load capacity due to its national road designation and increased traffic density. While its integrity has been diminished in recent years, it was originally a significant example of metal girder bridge construction and an example of the SRC's work in the 1930s. While bridges of this type dating from the late nineteenth century to the present are common in Maryland, Bridge No. 3001 originally featured distinguishing characteristics such as open balustrades and commemorative plaque. While the balustrades were replaced for safety considerations and the plaque removed for preservation, the bridge retains incised endposts and horizontally-incised abutments and wing walls that illustrate the SRC's designs of this era, as well as the concrete encased, rolled steel girders that are important elements of its historic engineering.

The route that contains Bridge No. 3001, leading north from Kingsville in Baltimore County to Lynch's Corner in Harford County, appears on maps as early as 1858. However, the main thoroughfare between Baltimore and Bel Air was the Belair Turnpike, also known as the Baltimore and Jerusalem Turnpike, which crossed Little Gunpowder Falls at Jerusalem Mill, a Harford County community approximately one mile southeast of Bridge No. 3001. The Belair Turnpike followed the route now called Jerusalem Road. The Belair Turnpike was authorized in 1791 and built through Kingsville in 1815. Kingsville is approximately two miles south of Bridge No. 3001, and is the location where the Belair Turnpike veered northeast to Jerusalem Mill. The Belair Turnpike fell into disrepair in the late nineteenth century, and in 1911, the turnpike company deeded the route to Baltimore County. Meanwhile, a steel highway bridge was present at the Bridge No. 3001 location as early as 1904, suggesting an increasing importance of the Kingsville to Lynch's Corner route.

The United States Department of Agriculture established a unified nationwide highway numbering system in 1925, approved the following year by the American Association of State Highway Officials (AASHO). The Department of Agriculture published a press release establishing US 1 on October 9, 1927. It described the route as the nation's most important interstate highway, connecting the Atlantic Seaboard's great manufacturing and industrial cities. By the 1930s, US 1 extended from Key West, Florida, to Fort Kent, Maine. Officials recognized it as the most eastward through-road on which all streams are bridged. US 1 roughly followed the automobile trail known as the Atlantic Highway and served as the principal tourist route extending from Maine to Florida. The highway also had strategic military value. In 1927, the press release boasted that the road conditions between Ellsworth, Maine, and Richmond, Virginia, were a dustless, all-year pavement for practically the entire distance of 884 miles. The press release also specified Maryland's portion of the route as, "Beginning at the Pennsylvania-Maryland State line south of Oxford via Rising Sun, Bel Air, Baltimore to the District of Columbia line."

In Maryland, communities rallied to have US 1 assigned to their roads, as the best routes between the locations specified by the federal government were debatable. Specifically, multiple routes connected Baltimore and Bel Air, and the Harford Turnpike was only

Maryland Historical Trust

Maryland Inventory of Historic Properties Form

Inventory No. BA-2718

Name
Continuation Sheet

Number 8 Page 1

slightly longer than the Belair Turnpike. However, road officials selected the Kingsville to Lynch's Corner Route, which by the 1930s was locally known as the Bel Air Road and described as having an excellent new, three-lane roadbed.

The SRC built Bridge No. 3001 in 1934. An endpost contained a plaque (now in the care of the Historical Society of Baltimore County) that documented its year of construction. G. Clinton Uhl was the SRC Chairman, and other members included E. Brooke Lee and Robert Lacy. The Chief Engineer was H.D. William, Jr., and W.C. Hopkins was the Bridge Engineer.

In the 1930s, an increase in motorized vehicle use and higher vehicle weight paralleled the growth of state-owned and state-aided highways. The SRC began to replace old, one-way bridges and other inadequate designs with standardized bridge plans, often steel-girder designs. Bridge No. 3001's four-lane width suggests heavy travel along this portion of US 1. At 68 feet long and four lanes wide, Bridge No. 3001 is too large to have been built using SRC standard plans, which were first developed in 1915 and periodically updated through the 1930s. Bridge No. 3001 does exhibit similar characteristics as standard plan bridges, such as horizontally-incised abutments and wing walls. It originally featured an open balustrade, which SRC standard plans first introduced in 1928.

Bridge No. 3001 is a common metal girder bridge. The SRC and the subsequent Maryland State Highway Administration (SHA), as well as local Maryland transportation agencies, have prolifically built metal girder bridges since the late nineteenth century to the present. However, when inventoried as part of SHA's Historic Bridge Inventory in 1995, the bridge retained distinguishing characteristics such open balustrades and commemorative plaque and was considered a significant example of its type. While the balustrades were replaced for safety considerations and the plaque removed for preservation, the bridge retains incised endposts and horizontally-incised abutments and wing walls that illustrate the SRC's designs of this era, as well as the concrete encased rolled steel girders that are important elements of its historic engineering.

The Interagency Historic Bridge Committee determined Bridge No. 3001 eligible for inclusion in the National Register of Historic Places (NRHP) in 2001 under Criterion C. Since that time, SHA replaced its deteriorated open balustrade with solid poured concrete parapets and removed its commemorative plaque, another character defining feature. Due to its deteriorated condition and failure to meet modern bridge standards, SHA's bridge system preservation program identified Bridge No. 3001 as structurally deficient and determined that bridge replacement was the best alternative for public safety. A Memorandum of Agreement between FHWA, MHT and SHA provided for the recordation of the bridge and the salvage of its bronze plaque which was donated to the Historical Society of Baltimore County. SHA began bridge replacement in the summer 2009. The new US 1 bridge over Little Gunpowder Falls opened to traffic in December 2009, restoring the four-lane travel pattern of two lanes in each direction. Construction work, including demolition of the old bridge, will continue in winter and spring 2010.

9. Major Bibliographical References

Inventory No. BA-2718

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Weingorff, Richard

2009 *From Names to Numbers: The Origins of the U.S. Numbered Highway System.* Federal Highway Administration Highway History. <http://wwwcf.fhwa.dot.gov/infrastructure/numbers.cfm> (accessed January 31, 2010).2009 *U.S. 1: Fort Kent, Maine to Key West, Florida.* Federal Highway Administration Highway History.<http://www.fhwa.dot.gov/highwayhistory/us1.cfm> (accessed January 31, 2010).

10. Geographical Data

Acreage of surveyed property	<u>approx. 0.07</u>
Acreage of historical setting	<u>approx. 0.07</u>
Quadrangle name	<u>White Marsh</u>

Quadrangle scale: 1:24,000

Maryland Historical Trust Maryland Inventory of Historic Properties Form

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Name
Continuation Sheet

Number 8 Page 1

Verbal boundary description and justification

The boundary encompasses Bridge No. 3001 and the ground on which it stands. The boundary isolates the bridge from adjacent areas that are not directly associated with the bridge's history.

11. Form Prepared by

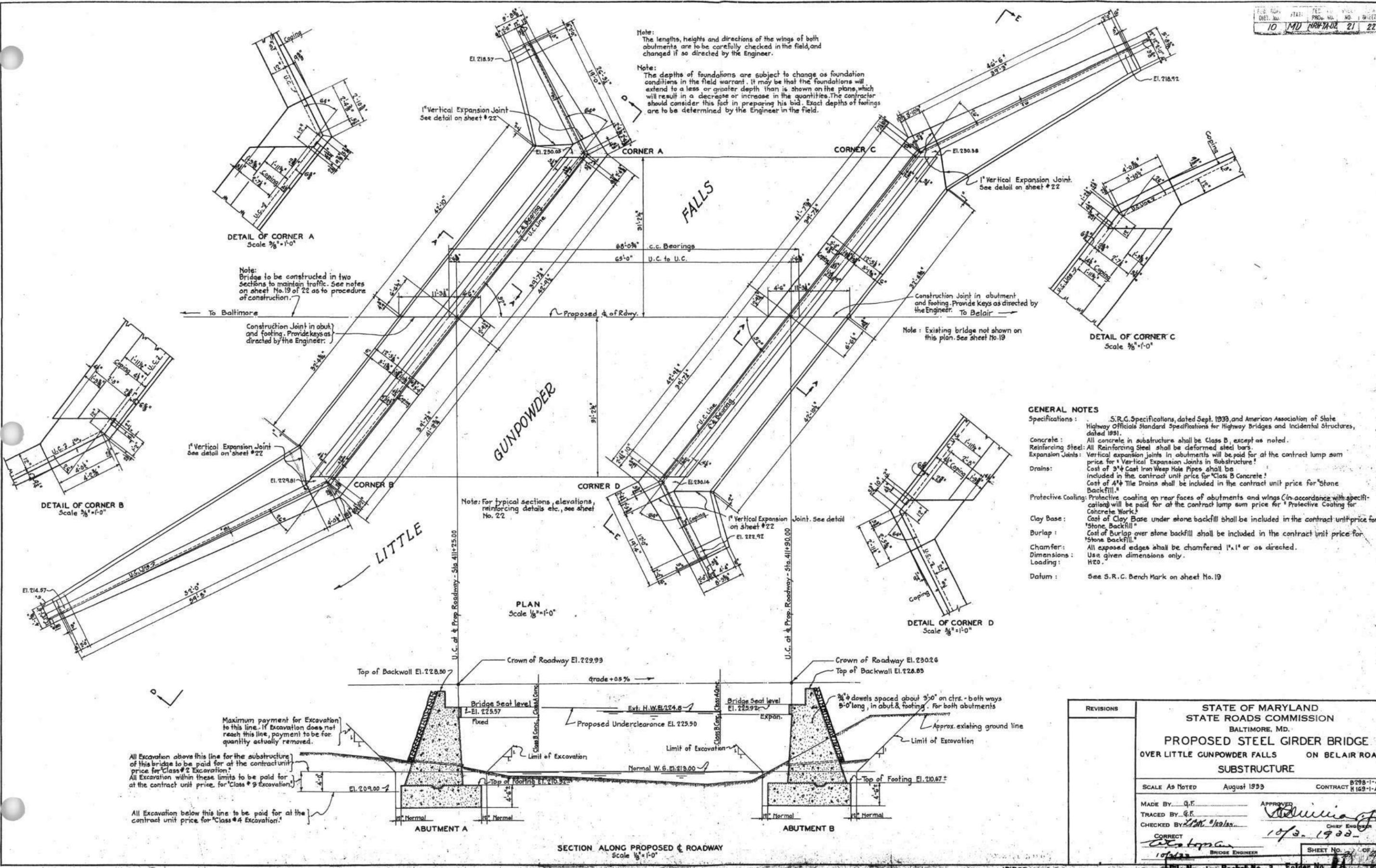
name/title	Rebecca Crew		
organization	Parsons-Brinckerhoff	date	February 4, 2010
street & number	100 S. Charles Street, Tower 1, 10 th Floor	telephone	410-727-5050
city or town	Baltimore	state MD	zipcode 21201

The Maryland Inventory of Historic Properties was officially created by an Act of the Maryland Legislature to be found in the Annotated Code of Maryland, Article 41, Section 181 KA, 1974 supplement.

The survey and inventory are being prepared for information and record purposes only and do not constitute any infringement of individual property rights.

return to: Maryland Historical Trust
Maryland Department of Planning
100 Community Place
Crownsville, MD 21032-2023
410-514-7600

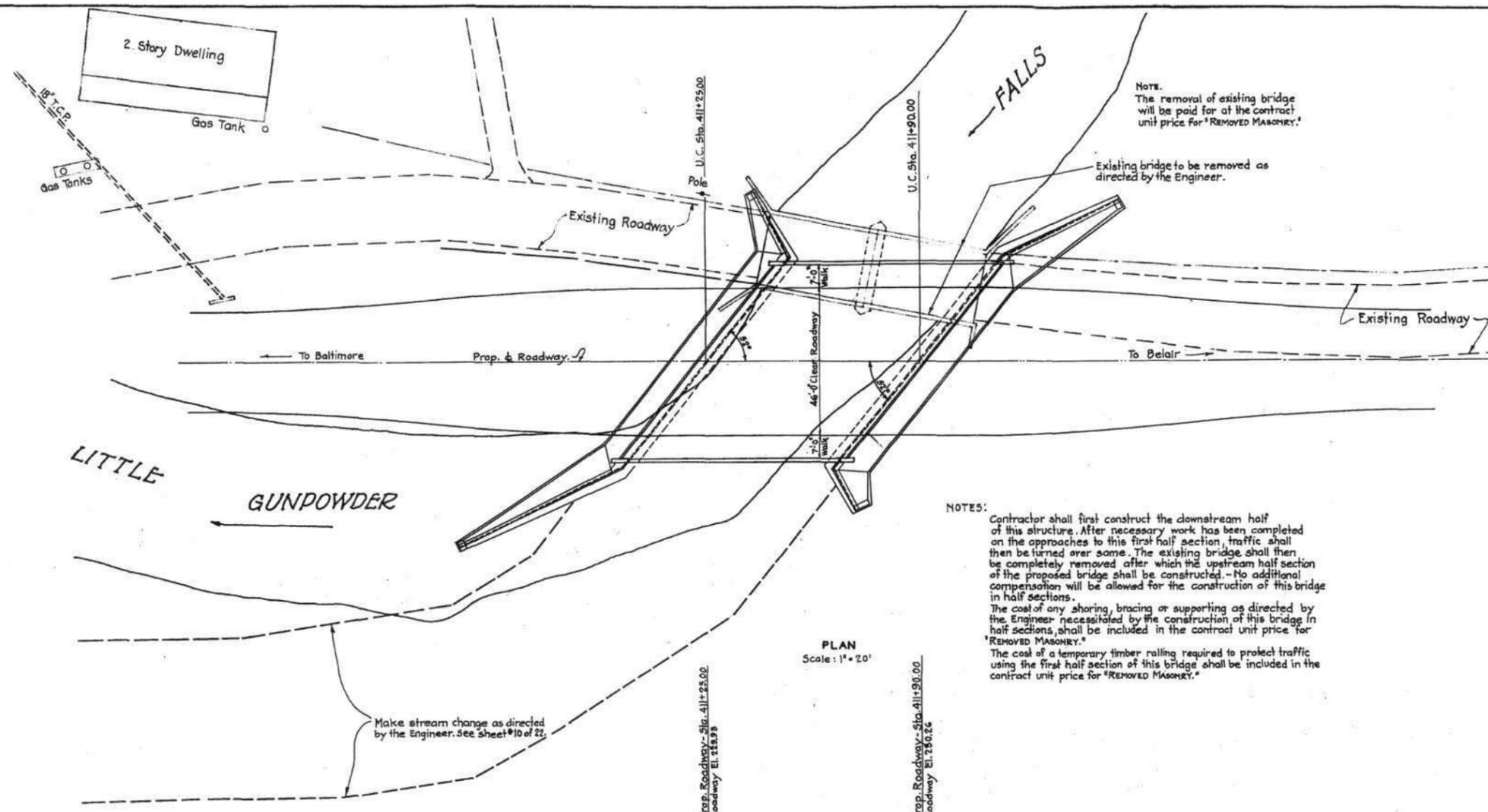
DATE	10	MD	NOV	21	22
PROJECT	MHP No. BA-2718				
SHEET	21 OF 22				



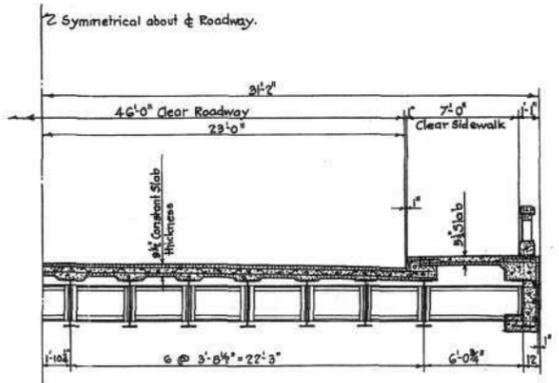
REVISIONS		STATE OF MARYLAND STATE ROADS COMMISSION BALTIMORE, MD.	
		PROPOSED STEEL GIRDER BRIDGE OVER LITTLE GUNPOWDER FALLS ON BELAIR ROAD SUBSTRUCTURE	
SCALE AS NOTED	August 1933	CONTRACT B 298-1-44	H 169-1-44
MADE BY: G.E.	TRACED BY: G.E.	CHECKED BY: <i>[Signature]</i>	APPROVED: <i>[Signature]</i> 10/2-1933 CHIEF ENGINEER
CORRECT		BRIDGE ENGINEER	
10/2/33		SHEET NO. 21 OF 22	
BRIDGE No. 3001		File No. ... Pocket No. ... Folder No. ...	

#5301 SHEET 21 OF 22

10 MD WAT-402 19 22

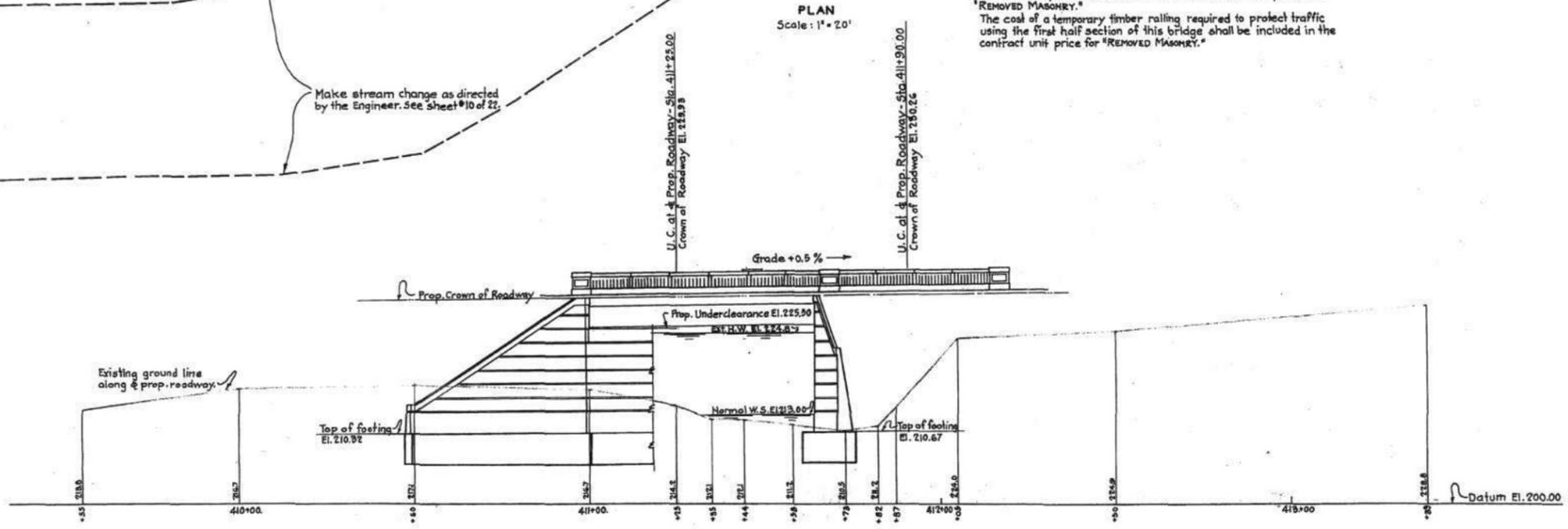


NOTES:
Contractor shall first construct the downstream half of this structure. After necessary work has been completed on the approaches to this first half section, traffic shall then be turned over same. The existing bridge shall then be completely removed after which the upstream half section of the proposed bridge shall be constructed. - No additional compensation will be allowed for the construction of this bridge in half sections.
The cost of any shoring, bracing or supporting as directed by the Engineer necessitated by the construction of this bridge in half sections, shall be included in the contract unit price for "REMOVED MASONRY."
The cost of a temporary timber railing required to protect traffic using the first half section of this bridge shall be included in the contract unit price for "REMOVED MASONRY."



DRAINAGE DATA

Drainage Area	= 29,000 Acres
Opening Required	= 552' - 736"
Existing Opening	= 508.0"
Proposed Opening	= 740.0"
Clear Span	= 65'-0"
Extreme H.W.	= El. 224.8 ±
Existing Underclearance	= El. 221.44
Proposed Underclearance	= El. 223.5



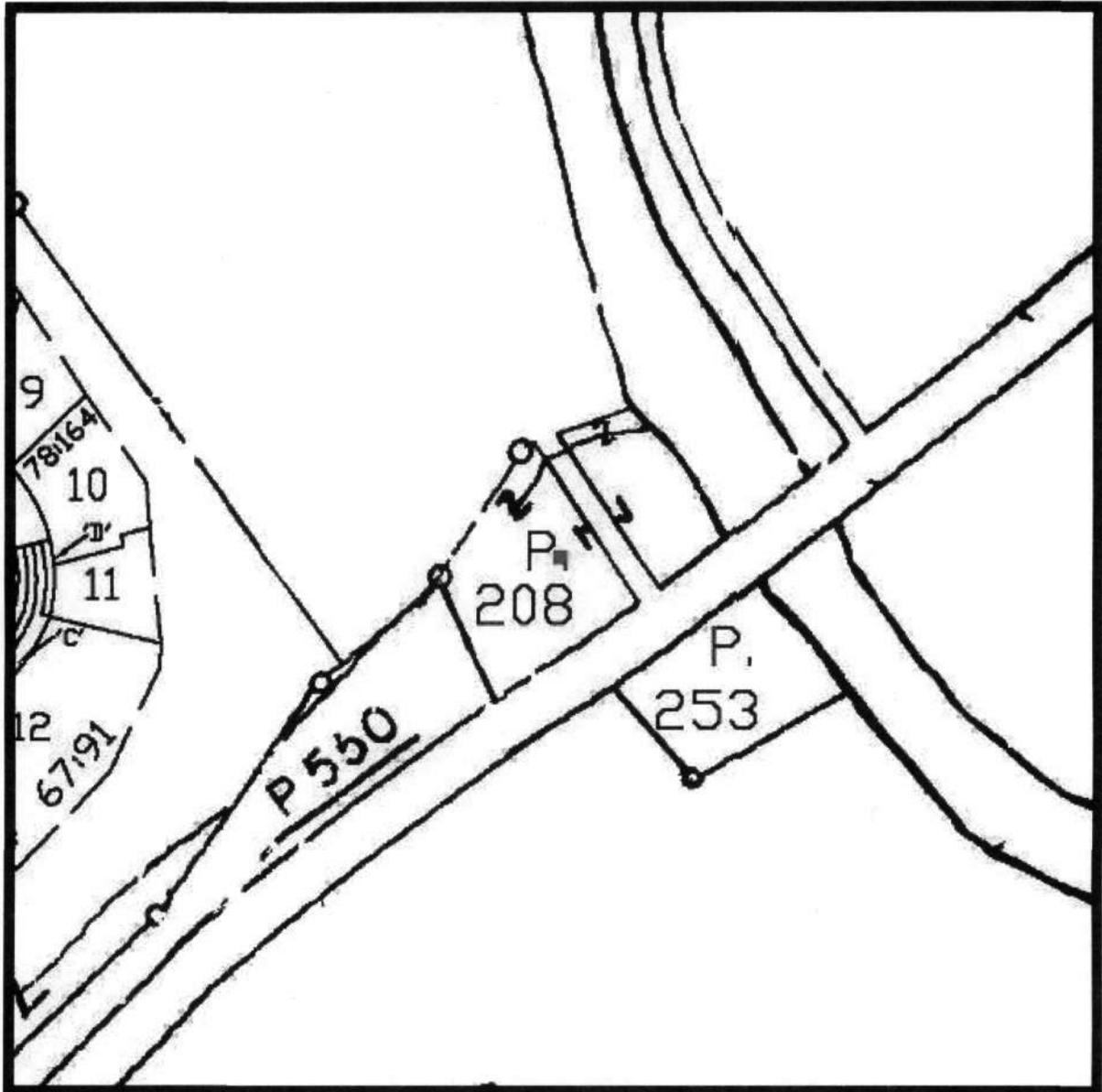
PROFILE
Scales: Horiz. 1" = 20'
Vert. 1" = 10'

Datum: Bench Mark No. 28 - 2 nails in Sycamore Tree left of sta. 411+20. (M.L.) El. 219.65

STATE OF MARYLAND STATE ROADS COMMISSION BALTIMORE, MD.	
PROPOSED STEEL GIRDER BRIDGE OVER LITTLE GUNPOWDER FALLS ON BELAIR ROAD PLAN & PROFILE	
SCALE As NOTED August 1933	CONTRACT B258-1 H169-1
MADE BY G.F.	APPROVED <i>[Signature]</i>
TRACED BY G.F.	CHIEF ENGINEER
CHECKED BY A.L.G.	1073-1923
CORRECT <i>[Signature]</i>	BRIDGE ENGINEER
107213	SHEET No. 10 OF 22

#3001 SHEET 10 OF 22

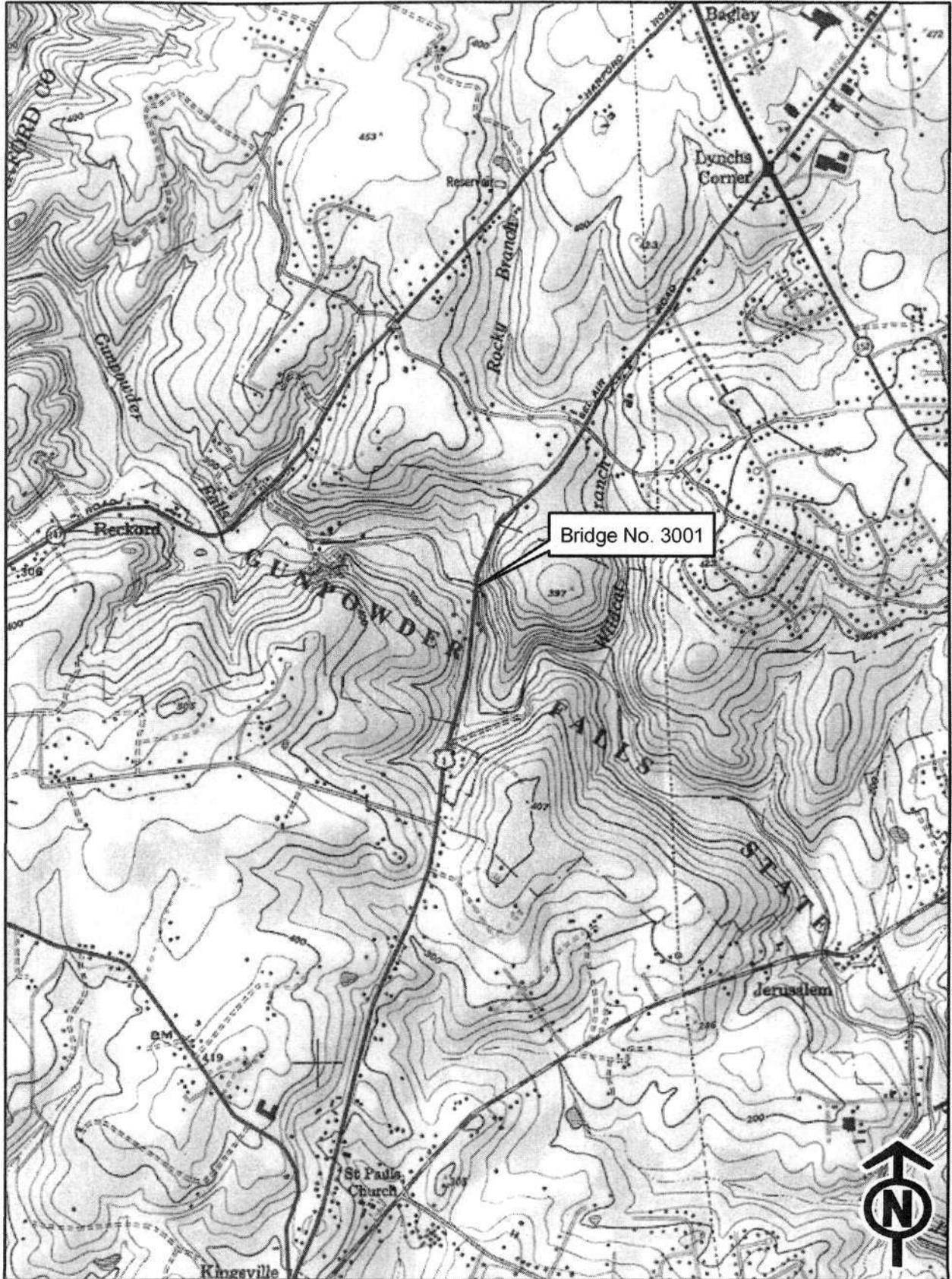
Bridge No. 3001 (BA-2718)
US 1 over Little Gunpowder Falls
Baltimore and Harford Counties
Tax Map 63; no parcel number



Bridge No. 3001 (BA-2718)
U.S. 1 over Little Gunpowder Falls
Border of Baltimore County and Harford County, MD

Bridge No. 3001 (BA-2718)

USGS White Marsh Quad, 1986



0 1,000 Feet

1:24,000



BA-2718

Bridge No. 3001

Baltimore County, Maryland

Fred Shoken, 2/9/2009

MD SHPO

View looking north from US 1 showing

west parapet

#1 of 8



BA-2718

Bridge No. 3001

Baltimore County, MD

Fred Shoken

2/9/2009

MD SHPO

East parapet south end (showing plaque removed)

2 of 8



BA-2718

Bridge No. 3001

Baltimore County, MD

Fred Shoken

2/9/2009

MD SHPO

West parapet

3 of 8



BA-2718

Bridge No. 3001

Baltimore County, MD

Fred Shoken

2/9/2009

MD SHPO

West parapet north end

4 of 8



BA-2718

Bridge No. 3001

Baltimore County, MD

Fred Shoken

2/9/2009

MD SHPO

View of north abutment and bridge superstructure

#5 of 8



BA-2718

Bridge No. 3001

Baltimore County, MD

Fred Shoken

2/9/2009

MD SHPO

West elevation from Little Gunpowder Falls

6 of 8



BA-2718

Bridge No. 3001

Baltimore County, MD

Fred Shoken

2/9/2009

MD SHPO

East elevation from Little Gunpowder Falls

7 of 8



BA-2718

Bridge No. 3001

Baltimore County, MD

Fred Shoken

2/9/2009

MD SHPO

Outer side east parapet

8 of 8

Maryland Historical Trust

Maryland Inventory of Historic Properties number: PH-2718

Name: US 1 over Little Gunpowder Falls

The bridge referenced herein was inventoried by the Maryland State Highway Administration as part of the Historic Bridge Inventory, and SHA provided the Trust with eligibility determinations in February 2001. The Trust accepted the Historic Bridge Inventory on April 3, 2001. The bridge received the following determination of eligibility.

MARYLAND HISTORICAL TRUST	
Eligibility Recommended <u> X </u>	Eligibility Not Recommended _____
Criteria: <u> A </u> <u> B </u> <u> X </u> <u> C </u> <u> D </u>	Considerations: <u> A </u> <u> B </u> <u> C </u> <u> D </u> <u> E </u> <u> F </u> <u> G </u> <u> None </u>
Comments: _____ _____ _____	
Reviewer, OPS: <u> Anne E. Bruder </u>	Date: <u> 3 April 2001 </u>
Reviewer, NR Program: <u> Peter E. Kurtze </u>	Date: <u> 3 April 2001 </u>

Handwritten signature

Maryland Inventory of Historic Properties
Historic Bridge Inventory
Maryland State Highway Administration
Maryland Historical Trust

MHT Number BA-2718

Name and SHA No. 3001

Location:

Street/Road Name and Number: US Route 1 over Little Gunpowder Falls

City/Town: Reckord Vicinity X

County: Baltimore

Ownership: X State County Municipal Other

This bridge projects over: Road Railway X Water Land

Is the bridge located within a designated district: yes X no

 NR listed district NR determined eligible district

 locally designated other

Name of District _____

Bridge Type:

 Timber Bridge

 Beam Bridge Truss-Covered Trestle

 Timber-and-Concrete

 Stone Arch

 Metal Truss

 Movable Bridge

 Swing Bascule Single Leaf Bascule Multiple Leaf

 Vertical Lift Retractable Pontoon

X Metal Girder

 Rolled Girder X Rolled Girder Concrete Encased

 Plate Girder Plate Girder Concrete Encased

 Metal Suspension

 Metal Arch

Metal Cantilever

Concrete

Concrete Arch Concrete Slab Concrete Beam

Rigid Frame

Other Type Name _____

Description:

Describe Setting:

Bridge Number 3001 carries U.S. 1 in a generally north-south direction over the Little Gunpowder Falls in the Baltimore County, Maryland. The approach to the roadway is level and has four lanes. The area around this bridge is wooded and rural. This area is part of the Gunpowder Falls State Park.

Describe Superstructure and Substructure:

Bridge number 3001 is a single span structure, measuring 68 feet in total length. The roadway width from curb to curb is 28 feet and the total deck width is 46 feet. There are sidewalks on both sides of the bridge and the width of each is four feet.

The superstructure is composed of a concrete encased steel rolled girder. There is one spans in the main bridge unit and no approach units. The span is 68 feet long. The floor system is composed of concrete cast-in-place. The joints are made of a preformed expansion material. There are two rectangular concrete parapets. The parapets are ornate with open balustrades, these are in poor condition. There is a historical plaque which commemorates the construction of this bridge in 1934.

The substructure is composed of concrete full height abutments the wing walls are also concrete. There are no piers or columns. There is no ornamentation. There are no historical plaques on the substructure.

The condition of this bridge is currently rated good, with some minor scour problems. The balustrade has deteriorated and is rated unacceptable and should be replaced.

Discuss Major Alterations: There have been no major alterations to this structure.

History:**When Built:** 1934**Why Built:** Increased traffic density necessitated a structure with an increased load capacity.**Who Built:** State Roads Commission**Why Altered:****Was this bridge built as part of an organized bridge building campaign:** Yes, a statewide road improvement program.**Surveyor Analysis:****This bridge may have NR significance for association with:** A Events Person C Engineering/Architectural**Was this bridge constructed in response to significant events in Maryland or local history:**

Increasing growth of vehicular traffic rates paralleled the growth of state-owned and state-aided highways. The 1930's brought a dramatic increase in the number of tractor-trailers and other heavy vehicles. The Maryland State Roads Commission began to emphasize standardized designs. Old, one way bridges and other inadequate designs were often replaced by steel girder design bridges.

When the bridge was built and/or given a major alteration, did it have a significant impact on the growth and development of the area?

Yes. Bridge 3001 had a significant impact. Belair Road (U.S. 1) has been an important north-south transportation route for centuries and a safe bridge across the Gunpowder Falls is vital. The ability to access the markets and employment potential of Baltimore City would have been seriously limited to locals had this bridge not been built. The steady outward growth of Baltimore City necessitated the steady growth of a sufficient transportation network. The construction of bridge 3001 would have been a significant part of this development. The neighborhoods of this area would have all been directly impacted.

Is the bridge located in an area which may be eligible for historic designation and would the bridge add to or detract from historic and visual character of the possible district?

Yes. Bridge 3001 is located in an area that has had an important and significant impact on the history of Baltimore, Maryland. The Gunpowder Falls area is a vital segment of Baltimore County history. This structure served both these neighborhoods. Several areas along Gunpowder Falls already are eligible for historic designation and the expansion of any or all of these areas might would entail the inclusion of this bridge. The loss of this bridge would negatively impact the historic and visual significance of these areas.

Is the bridge a significant example of its type?

Yes. Bridge 3001 is a common type of metal girder bridge. Metal girder bridges were built prolifically in Maryland from the late nineteenth century to the present day. There is nothing to set this bridge apart from others of its type. There are numerous other examples of this bridge available.

Does the bridge retain integrity of the important elements described in the Context Addendum?

Yes. Bridge Number 3001 does retain important elements of its historical structural integrity. The primary character defining elements are the concrete encased rolled steel girders and concrete abutments.

Should this bridge be given further study before significance analysis is made and Why?

Yes. Bridge 3001 should be studied further to determine its eligibility for the National Register. A Significance analysis should be made following the National Register Criteria for Evaluation.

Under criteria A, Bridge 3001 should be studied in the context of its historical significance. This bridge can be associated with the development of the neighborhoods of Gunpowder Falls. Further study should be made to determine its significance to the pattern of events and trends toward urbanization and industrialization that are characterized by the era of its construction. A determination of the significance of its location should include the nature and origin of the property it is constructed on. This should include previous structures and the history of that area as a crossing.

Under criteria C, the distinctive characteristics of this bridge should be studied to include the type, period, and method of construction.

Under criteria D, the potential for information of Bridge 3001 should be studied further. This structure was built during a period of intense urbanization and industrialization in Maryland and the country as a whole.

Bibliography:

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Surveyor:

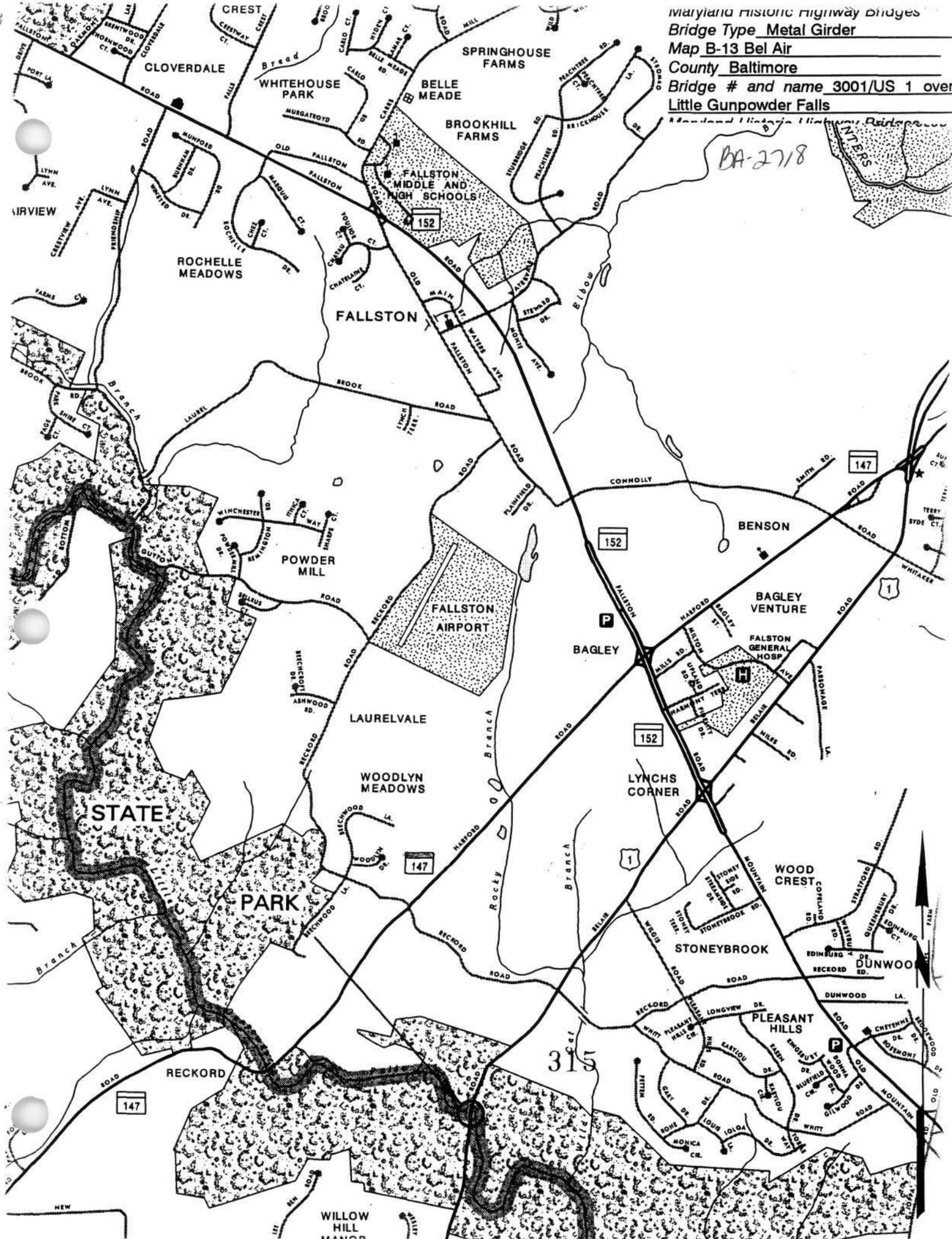
Name: Andrew M. Watts **Date:** March 1996

Organization: State Highway Administration **Telephone:** (410) 321-2213

Address: 2323 West Joppa Road, Brooklandville, MD 21022

Maryland Historic Highway Bridges
Bridge Type Metal Girder
Map B-13 Bel Air
County Baltimore
Bridge # and name 3001/US 1 over Little Gunpowder Falls

BA-2718



315



Inventory # BA-2718

Name 3001-US1 OVER LITTLE GUNPOWDER FALLS

County/State BALTIMORE COUNTY/MD

Name of Photographer DAVE DIEHL

Date 1/95

Location of Negative SHA

Description SOUTH APPROACH LOOKING
NORTHEAST

Number 115 of 206



Inventory # BA-2718

Name 3001-US1 OVER LITTLE GUNPOWDER FALLS

County/State BALTIMORE COUNTY/MO

Name of Photographer DAVE DIEHL

Date 1/95

Location of Negative SHA

Description WEST ELEVATION LOOKING
SOUTHEAST

Number ~~10~~ ² of ~~30~~ ³⁶



Inventory # BA-2718

Name 3001-US1 OVER LITTLE GUN POWDER FALLS

County/State BALTIMORE COUNTY/MD

Name of Photographer DAVE DIEHL

Date 1/95

Location of Negative SWA

Description EAST ELEVATION LOOKING
NORTH

Number 3 of 306

LITTLE GUNPOWDER FALLS BRIDGE

BUILT - 1934

STATE ROADS COMMISSION

C. CLINTON UHL - CHAIRMAN

E. BROOKE LEE. ROBERT LACY

H. D. WILLIAMS, JR. - CHIEF ENGINEER

W. C. HOPKINS - BRIDGE ENGINEER

Inventory # BA-2718

Name 3001- US1 OVER LITTLE GUNPOWDER FALLS

County/State BALTIMORE COUNTY/MD

Name of Photographer DAVE DIEHL

Date 1/95

Location of Negative SHA

Description PLAQUE ON WEST PARAPET

Number 4 of 6



Inventory # BA-2718

Name 3001- US1 OVER LITTLE GUNPOWDER FALLS

County/State BALTIMORE COUNTY MD

Name of Photographer DAVE DIEHL

Date 1/95

Location of Negative SHA

Description ALTERED EAST PARAPET

Number 5 of 30



Inventory # BA-2718

Name 3001-US1 OVER LITTLE GUNPOWDER FALLS

County/State BALTIMORE COUNTY/MD

Name of Photographer DAVE DIEHL

Date 1/95

Location of Negative SHA

Description NORTH APPROACH LOOKING

SOUTHWEST

Number 6 of 6
20 of 30