

Maryland Historical Trust

Maryland Inventory of Historic Properties number: F-2-92

Name: 10089/MD 478 over a branch of the Potomac River

The bridge referenced herein was inventoried by the Maryland State Highway Administration as part of the Historic Bridge Inventory, and SHA provided the Trust with eligibility determinations in February 2001. The Trust accepted the Historic Bridge Inventory on April 3, 2001. The bridge received the following determination of eligibility.

MARYLAND HISTORICAL TRUST	
Eligibility Recommended <u> X </u>	Eligibility Not Recommended <u> </u>
Criteria: <u> A </u> <u> B </u> <u> C </u> <u> D </u>	Considerations: <u> A </u> <u> B </u> <u> C </u> <u> D </u> <u> E </u> <u> F </u> <u> G </u> <u>None</u>
Comments: _____ _____	
Reviewer, OPS: <u> Anne E. Bruder </u>	Date: <u> 3 April 2001 </u>
Reviewer, NR Program: <u> Peter E. Kurtze </u>	Date: <u> 3 April 2001 </u>

MARYLAND INVENTORY OF HISTORIC PROPERTIES
HISTORIC BRIDGE INVENTORY
MARYLAND STATE HIGHWAY ADMINISTRATION
MARYLAND HISTORICAL TRUST

MHT NO. F-2-92

NAME AND SHA NO.: 10089

LOCATION

Road Name and Number: MD 478 over a branch of the Potomac River

City/Town: Brunswick _ vicinity

County: Frederick

Ownership: State _ County _ Municipal _ Other

Bridge projects over: _ Road _ Railway Water _ Land

Is bridge located within designated district?: yes _ no

NR listed district _ NR determined eligible district

locally designated _ other

Name of District Brunswick National Historic Register District

BRIDGE TYPE

Timber Bridge

Beam Bridge Truss-Covered Trestle Timber-and-Concrete

Stone Arch Bridge

Metal Truss Bridge

Moveable Bridge

Swing Bascule Single Leaf Bascule Multiple Leaf

Vertical Lift Retractable Pontoon

Metal Girder

Rolled Girder Rolled Girder Concrete Encased

Plate Girder Plate Girder Concrete Encased

Metal Suspension

Metal Arch

Metal Cantilever

Concrete

Concrete Arch Concrete Slab Concrete Beam Rigid Frame

Other Type Name

DESCRIPTION

Describe the Setting:

Bridge #10089 carries MD 478 over a branch of the Potomac River in Frederick County. This portion of MD 478 is a designated scenic route, and is located near the border between Maryland's Piedmont and Appalachian physiographic regions. Route 478 generally runs in an east-west direction. The C & O Canal National Historical Park is in the vicinity of the bridge. The bridge is located in a residential area.

Describe the Superstructure and Substructure: (Discuss points identified in Context Addendum, Section C)

Bridge #10089 carries two lanes of traffic over a branch of the Potomac River. The structure is a single-span bridge with a span length of 25' and a clear roadway width of 24'. The bridge is constructed of 5 concrete girders, plain concrete wingwalls and abutments, and solid panelled concrete parapets. Modern metal guardrails flank both approaches and are attached to the ends of the parapets but do not extend along the inside face of the parapets.

A survey of historic concrete beam bridges undertaken by the Maryland State Highway Administration in the Fall of 1995 identified 113 bridges of that type located throughout the state. Slightly more than two-thirds (76) of that total were single-span bridges.

Discuss major alterations:

No record of any major alterations was located in the bridge inspection and drawing files at the State Highway Administration.

HISTORY

When Built: 1925

Why Built: Statewide road improvement programs and local transportation needs

Who Built: State Roads Commission, contract # F 83

Who Designed: Unknown; built according to standard state specifications

Why Altered: N/A

Was this bridge built as part of an organized bridge building campaign?: No

SURVEYOR ANALYSIS

This bridge may have NR significance for association with:

A (Events) B (Person) C (Engineering/Architectural Character)

Was this bridge constructed in response to significant events in Maryland or local history?

Road improvements in Frederick County were fueled by several events occurring during the early twentieth century. First, the Good Roads Movement, which began in the last decade of the nineteenth century, aimed to improve primary roads throughout the state as well as multiple connecting roads between counties. As the movement progressed, numerous existing roads were widened, straightened, or graded, and many new bridges were built to carry the rebuilt roads. Second, rapidly increasing automobile, truck, and bus traffic also fueled the replacement of existing narrow and weak bridges with wider and stronger concrete structures, many of which were built according to standardized specifications and plans developed by the State Roads Commission (SRC). Third, the State Roads Commission established district engineering offices during the 1910s to aid in intrastate road development, and established a separate bridge department in 1920. This fostered construction of many concrete bridges throughout the state. In the 1920s, the SRC emphasized improving the safety and comfort of primary routes while developing secondary networks and feeder roads. By the 1930s, bridges that were originally deemed adequate had become unacceptable for carrying modern traffic loads and many new structures were built as a result.

When the bridge was built, and/or given a major alteration, did it have a significant impact on the growth and development of the area?

Bridge #10089 participated in the general trend toward upgrading state roads and bridges and improving intrastate access.

Is the bridge located in an area which may be eligible for historic designation, and would the bridge add or detract from the historic and visual character of the possible district?

Bridge #10089 may contribute to the Brunswick National Register Historic District where it is located.

Is the bridge a significant example of its type?

No, this bridge is not a significant example of its type.

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Does the bridge retain integrity of the important elements described in the Context Addendum?

Yes, this bridge retains integrity of its character-defining elements. The character-defining elements for the superstructures of concrete beam bridges are the slab, the longitudinal beams, and the parapet or railing when integral. For the substructure, the character-defining elements are the abutments, piers, and wing walls. No record of any major alterations could be located.

Is the bridge a significant example of the work of the manufacturer, designer, and/or engineer, and why?

No, this structure is not a significant example of the work of the State Roads Commission.

Should this bridge be given further study before significance analysis is made, and why?

Yes. Further study should indicate whether this bridge contributes to the Brunswick National Register Historic District.

BIBLIOGRAPHY

Spero, P.A. C. & Company and Louis Berger & Associates
1994 *Historic Bridges in Maryland: Historic Context Report.*
 Maryland State Highway Administration, Baltimore.

State Highway Administration
 Bridge Inspection Reports. On file 707 North Calvert Street, Baltimore.

 As-Built Drawings. On file 707 North Calvert Street, Baltimore.

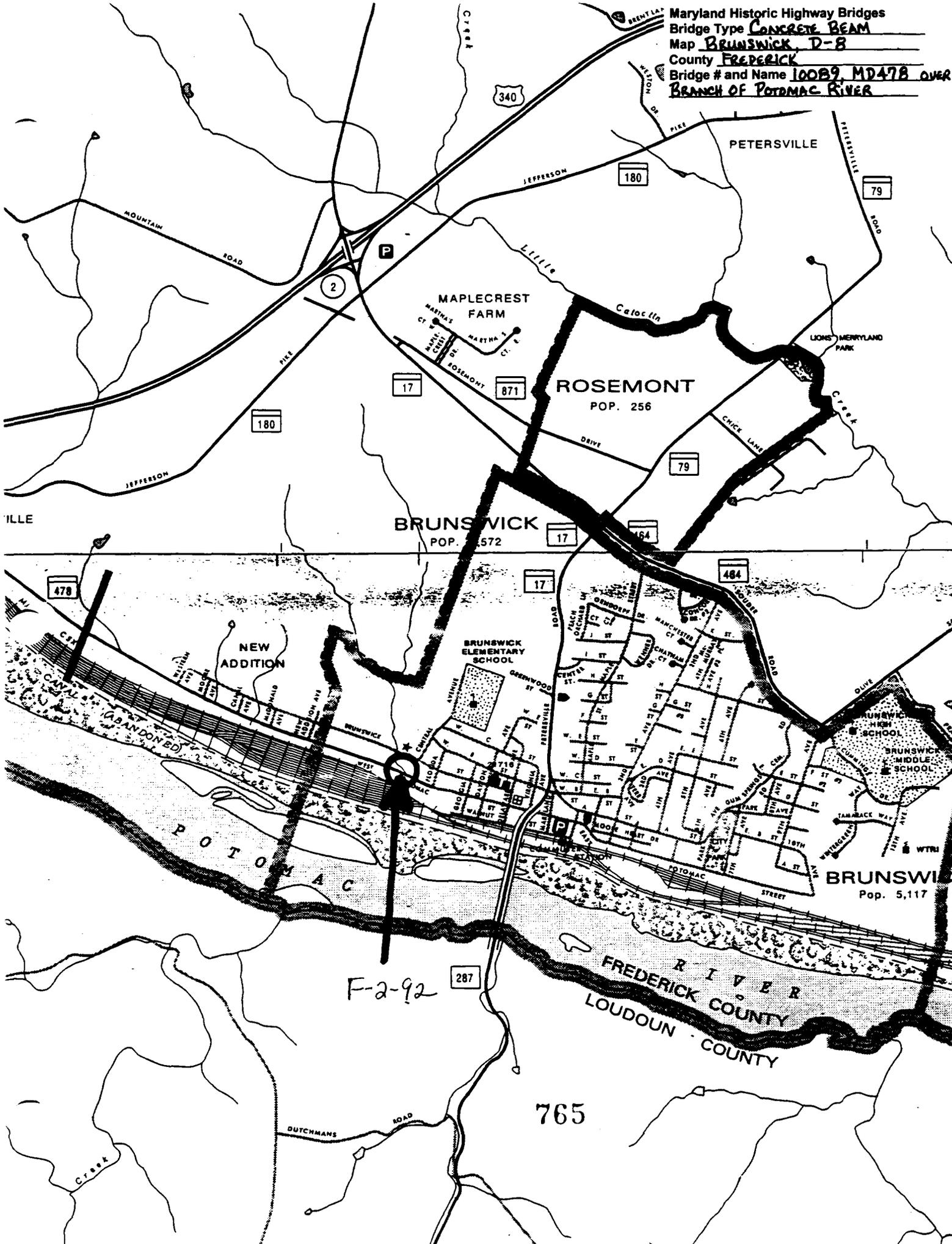
State Roads Commission of Maryland
1958 *A History of Road Building in Maryland.* Baltimore.

SURVEYOR INFORMATION

Name: Gabrielle M. Lanier/Stephen Linhart
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 Mechanicsburg, PA 17055

Date: 13 May 1996
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Maryland Historic Highway Bridges
Bridge Type CONCRETE BEAM
Map BRUNSWICK, D-8
County FREDERICK
Bridge # and Name 100B9, MD478 OVER
BRANCH OF POTOMAC RIVER



F-2-92

765



Inventory # F-2-92

Name 10089-MD478 OVER A BRANCH OF POTOMAC RIVER

County/State FREDERICK COUNTY / MD

Name of Photographer FRANK JULIANO

Date 1/95

Location of Negative SWA

Description WEST APPROACH

Number 1 of 34 4



**SCENIC
ROUTE**

Inventory # F-2-92

Name 10089-MD478 OVER A BRANCH OF POTOMAC RIVER

County/State FREDERICK COUNTY/MD

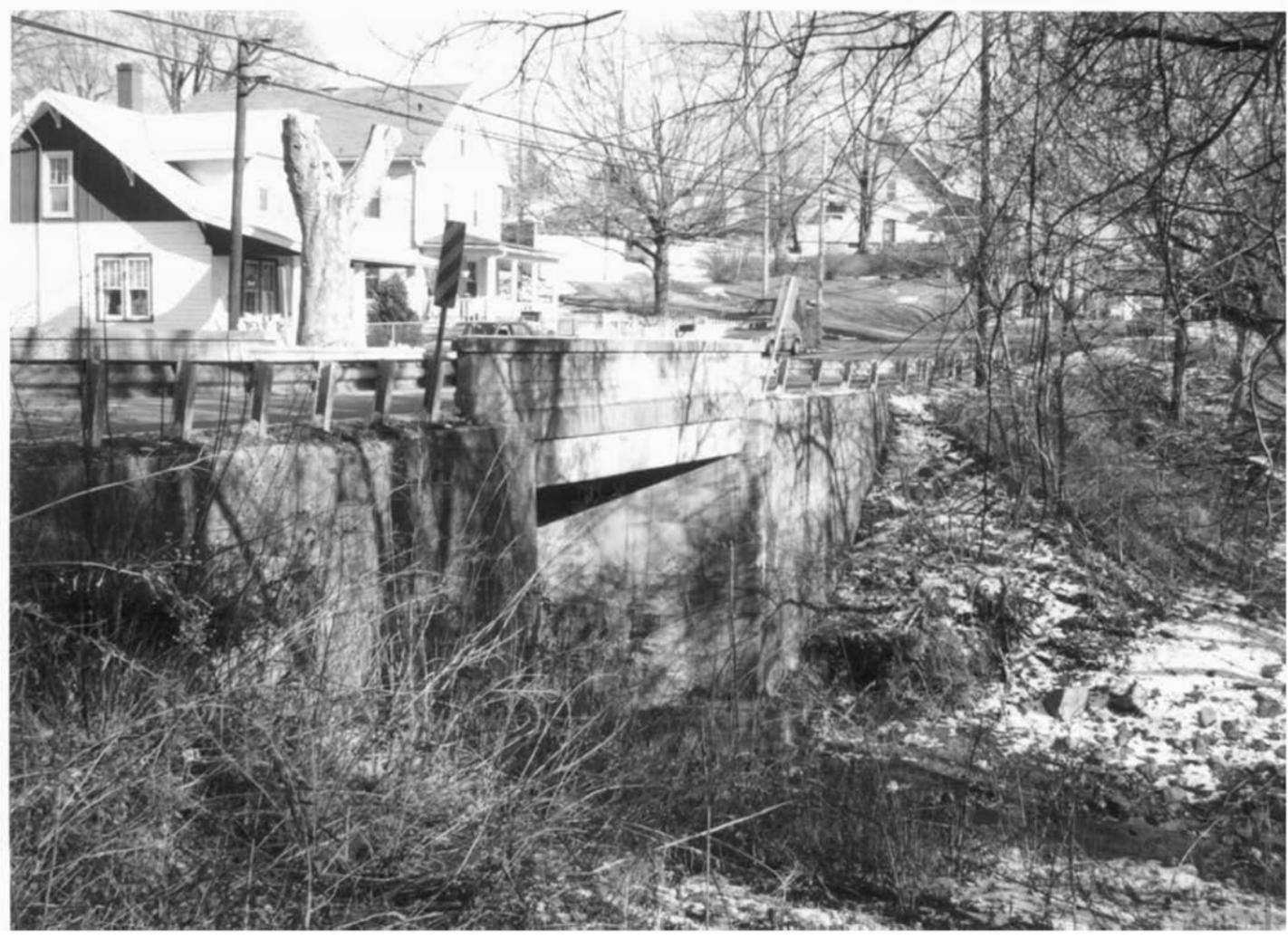
Name of Photographer FRANK JULIANO

Date 1/95

Location of Negative SHA

Description EAST APPROACH

Number 2 of 34



Inventory # F-2-92

Name W089-MD478 OVER A BRANCH OF POTOMAC RIVER

County/State FREDERICK COUNTY MD

Name of Photographer FRANK JULIANO

Date 1/95

Location of Negative SHA

Description ELEVATION LOOKING NORTH

Number 3 of 34



Inventory # F-2-92

Name W089-MD478 OVER A BRANCH OF POTOMAC RIVER

County/State FREDERICK COUNTY/MD

Name of Photographer FRANK JULIANO

Date 1/95

Location of Negative SNA

Description ELEVATION LOOKING SOUTH

Number 49 of 344