

Maryland Historical Trust

Maryland Inventory of Historic Properties number: HO 655  
Name: HO 110 / Henryton Below Patuxent River Trib

The bridge referenced herein was inventoried by the Maryland State Highway Administration as part of the Historic Bridge Inventory, and SHA provided the Trust with eligibility determinations in February 2001. The Trust accepted the Historic Bridge Inventory on April 3, 2001. The bridge received the following determination of eligibility.

MARYLAND HISTORICAL TRUST	
Eligibility Recommended _____	Eligibility Not Recommended <u>X</u>
Criteria: <u>  </u> A <u>  </u> B <u>  </u> C <u>  </u> D	Considerations: <u>  </u> A <u>  </u> B <u>  </u> C <u>  </u> D <u>  </u> E <u>  </u> F <u>  </u> G <u>  </u> None
Comments: _____ _____ _____	
Reviewer, OPS: <u>Anne E. Bruder</u>	Date: <u>3 April 2001</u>
Reviewer, NR Program: <u>Peter E. Kurtze</u>	Date: <u>3 April 2001</u>

MARYLAND INVENTORY OF HISTORIC BRIDGES  
HISTORIC BRIDGE INVENTORY  
MARYLAND STATE HIGHWAY ADMINISTRATION/  
MARYLAND HISTORICAL TRUST

MHT No. HO-655

SHA Bridge No. HO 110 Bridge name Henryton Road over Patapsco River Tributary

**LOCATION:**

Street/Road name and number [facility carried] Henryton Road

City/town Sykesville Vicinity X

County Howard

This bridge projects over: Road \_\_\_ Railway \_\_\_ Water X Land \_\_\_

Ownership: State \_\_\_ County X Municipal \_\_\_ Other \_\_\_

**HISTORIC STATUS:**

Is the bridge located within a designated historic district? Yes \_\_\_ No X

National Register-listed district \_\_\_ National Register-determined-eligible district \_\_\_

Locally-designated district \_\_\_ Other \_\_\_

Name of district \_\_\_\_\_

**BRIDGE TYPE:**

Timber Bridge \_\_\_:

Beam Bridge \_\_\_ Truss -Covered \_\_\_ Trestle \_\_\_ Timber-And-Concrete \_\_\_

Stone Arch Bridge \_\_\_\_\_

Metal Truss Bridge \_\_\_\_\_

Movable Bridge \_\_\_:

Swing \_\_\_ Bascule Single Leaf \_\_\_ Bascule Multiple Leaf \_\_\_

Vertical Lift \_\_\_ Retractable \_\_\_ Pontoon \_\_\_

Metal Girder \_\_\_\_\_:

Rolled Girder \_\_\_ Rolled Girder Concrete Encased \_\_\_\_\_

Plate Girder \_\_\_ Plate Girder Concrete Encased \_\_\_\_\_

Metal Suspension \_\_\_\_\_

Metal Arch \_\_\_\_\_

Metal Cantilever \_\_\_\_\_

Concrete X:

Concrete Arch \_\_\_ Concrete Slab X Concrete Beam \_\_\_ Rigid Frame \_\_\_

Other \_\_\_ Type Name \_\_\_\_\_

**DESCRIPTION:**

Setting: Urban \_\_\_\_\_ Small town \_\_\_\_\_ Rural  X

**Describe Setting:** Bridge No. HO 110 carries Henryton Road over Patapsco River Tributary in Howard County. Henryton Road runs north-south, while the tributary of the Patapsco River flows from the east to the west. The area around the bridge is forested with a few buildings within the vicinity of the bridge.

**Describe Superstructure and Substructure:**

Bridge No. HO 110 over a Tributary of the Patapsco River in Howard County is a single span concrete slab bridge built c. 1930. The clear span length is 19'-6", the total bridge length is 21' and the clear roadway width is 27'. The bridge is not currently posted. The bridge is skewed at a 16 degree angle to the roadway centerline. The superstructure consists of the roadway, slab and railing. The roadway surface has a large patch on the east side of the bridge roadway. The slab has approximately 4' of fill between the concrete deck and the road surface. The west fascia has longitudinal cracking, while the east fascia has areas of spalling and efflorescence at the deck/curb intersection. This area has been previously patched. The parapets were replaced with w-beam guardrails at an unknown date. The substructure consists of the concrete abutments and wingwalls. The west wingwalls are parallel with the abutment center line, while the east abutments are flared.

**Discuss Major Alterations:**

Four feet of fill was placed on top of the slab and the concrete parapets were replaced with w-beam guardrails. Both alterations occurred at an unknown date.

**HISTORY:**

**WHEN was the bridge built:** 1930

**This date is:** Actual \_\_\_\_\_ Estimated  X

**Source of date:** Plaque \_\_\_\_\_ Design plans \_\_\_\_\_ County bridge files/inspection form  X

**Other (specify)**

**WHY was the bridge built?**

Maryland's primary and secondary roads and bridges had become inadequate to the huge trucks and volumes of cars in use after World War I.

**WHO was the designer?**

Unknown

**WHO was the builder?**

Unknown

**WHY was the bridge altered?**

The bridge was altered to extend the life of the bridge.

**Was this bridge built as part of an organized bridge-building campaign?**

Yes, post World War I improvements to primary and secondary roads.

**SURVEYOR/HISTORIAN ANALYSIS:**

**This bridge may have National Register significance for its association with:**

- A - Events \_\_\_\_\_
- B- Person \_\_\_\_\_
- C- Engineering/architectural character \_\_\_\_\_

The bridge does not have National Register significance.

**Was the bridge constructed in response to significant events in Maryland or local history?**

Maryland's roads and bridge improvement programs mirrored economic cycles. The first road improvement of the State Roads Commission was a 7 year program, starting with the Commissions establishment in 1908 and ending in 1915. Due to World War I, the period from 1916-1920 was one of relative inactivity; only roads of first priority were built. Truck traffic resulting from war related factories and military installations generated new, heavy traffic unanticipated by the builders of the early road system. From 1920-1929, numerous highway improvements occurred in response to the increase in Maryland motor vehicles from 103,000 in 1920 to 320,000 in 1929, with emphasis on the secondary system of feeder roads which moved traffic from the primary roads built before World War I. After World War I, Maryland's bridge system also was appraised as too narrow and structurally inadequate for the increasing traffic, with plans for an expanded bridge program to be handled by the Bridge Division, set up in 1920. In 1920 under Chapter 508 of the Acts of 1920 the State issued a bond of \$3,000,000.00 for road construction; the primary purpose of these monies was to meet the state obligations involving the construction of rural post roads. The secondary purpose of these monies was to fund (with an equal sum from the counties) the building of lateral roads. the number of hard surfaced roads on the state system grew from 2000 in 1920 to 3200 in 1930. By 1930, Maryland's primary system had been inadequate to the huge freight trucks and volume of passenger cars in use, with major improvements occurring in the late 1930's. Most improvements to local roads waited until the years after World War II.

**When the bridge was built and/or given a major alteration, did it have a significant impact on the growth and development of the area?**

No, this bridge did not have a direct impact on the growth or development of the area.

**Is the bridge located in an area which may be eligible for historic designation and would the bridge add to or detract from the historic/visual character of the potential district?**

No, the bridge is not located in an area which may be eligible for historic designation.

**Is the bridge a significant example of its type?**

No, this bridge is not a significant example of its type because its character defining features have been altered or they are in a deteriorated state.

**Does the bridge retain integrity of important elements described in Context Addendum?**

No, this structure does not retain the integrity of its original design because the parapets were removed.

**Is the bridge a significant example of the work of a manufacturer, designer, and/or engineer?**

Unknown

**Should the bridge be given further study before an evaluation of its significance is made?**

No, this structure should not be given further study. Its current condition has placed its integrity in doubt.

**BIBLIOGRAPHY:**

County inspection/bridge files     X          SHA inspection/bridge files                       
Other (list):

**SURVEYOR:**

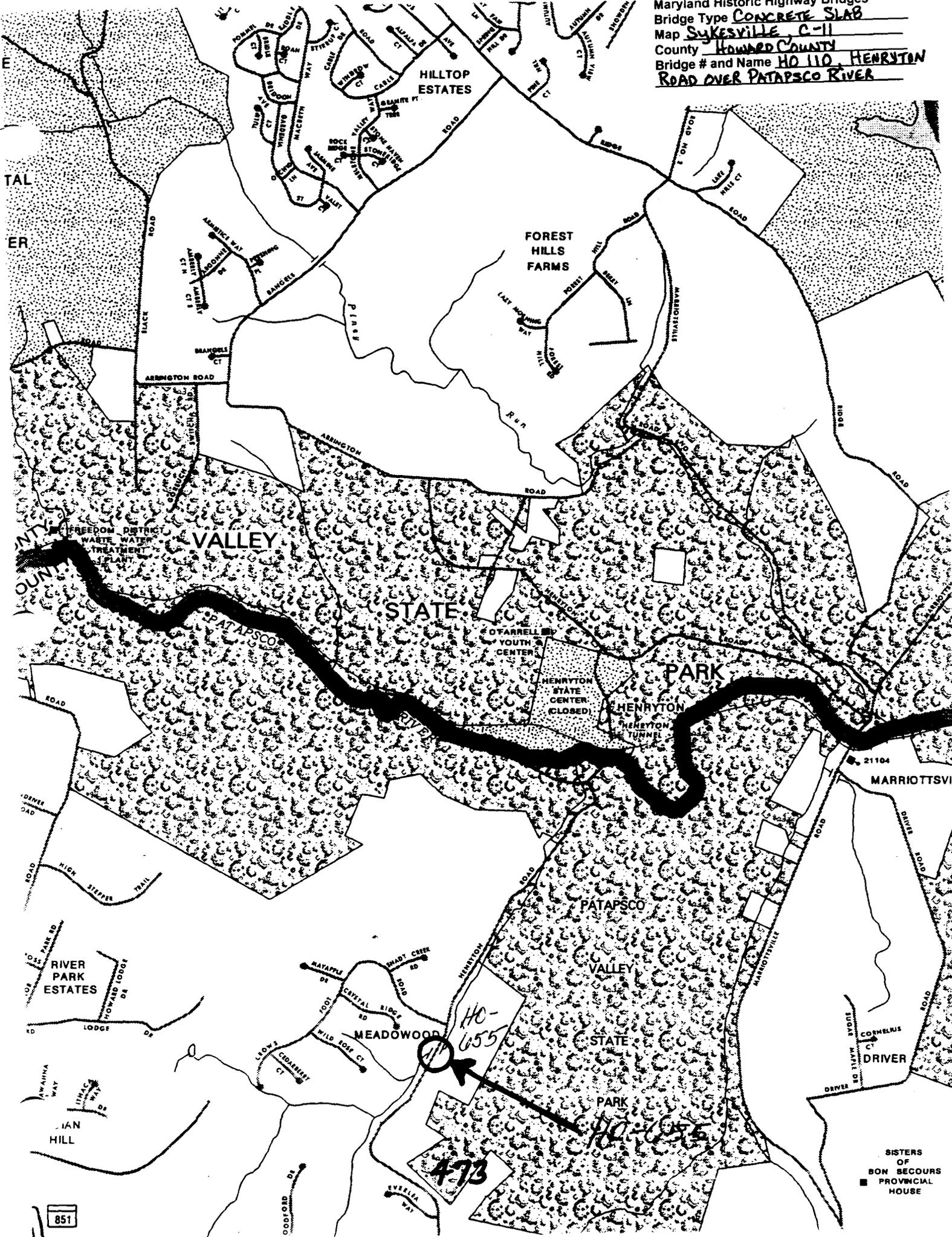
Date bridge recorded     8/95    

Name of surveyor Leo Hirrell

Organization/Address P.A.C. Spero & Company, 40 W. Chesapeake Avenue, Suite 412, Baltimore, MD 21204

Phone number (410) 296-1635      FAX number (410) 296-1670

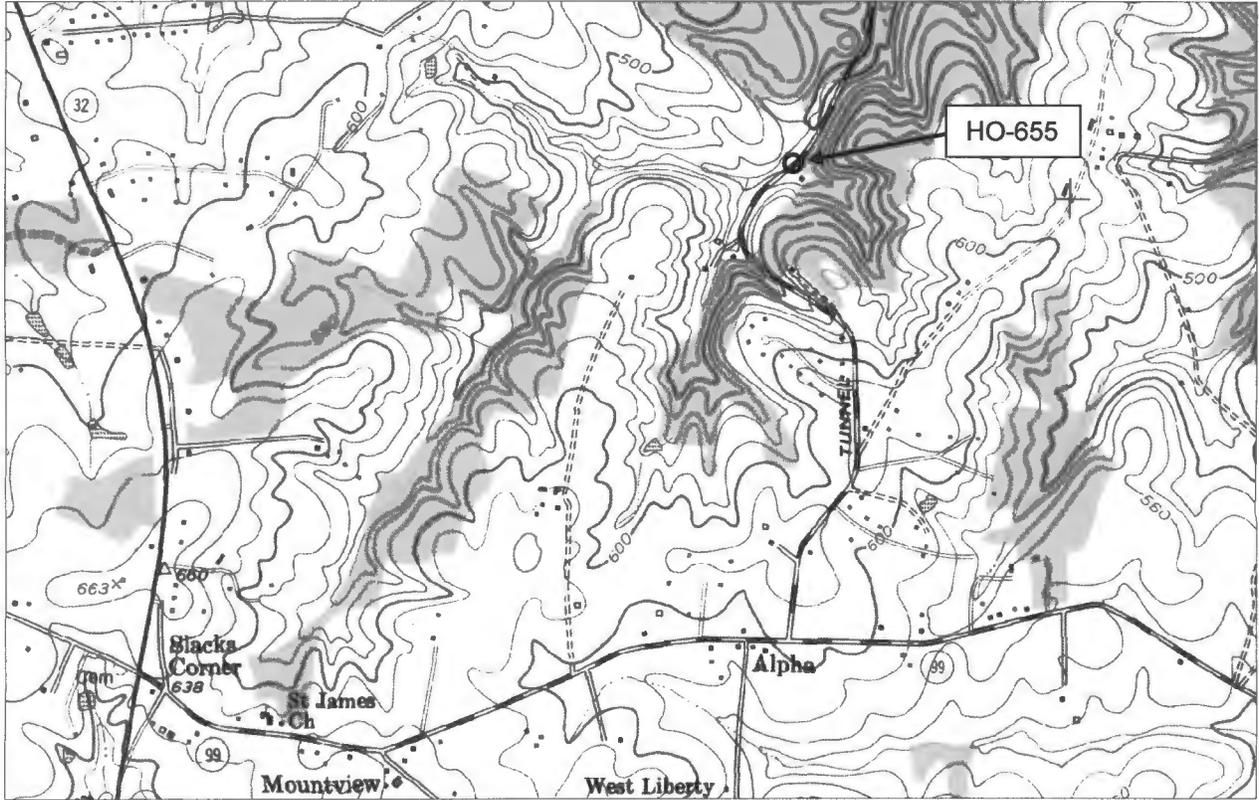
Maryland Historic Highway Bridges  
 Bridge Type CONCRETE SLAB  
 Map SYKESVILLE, C-11  
 County HOWARD COUNTY  
 Bridge # and Name HO 110, HENRYTON ROAD OVER PATAPSCO RIVER



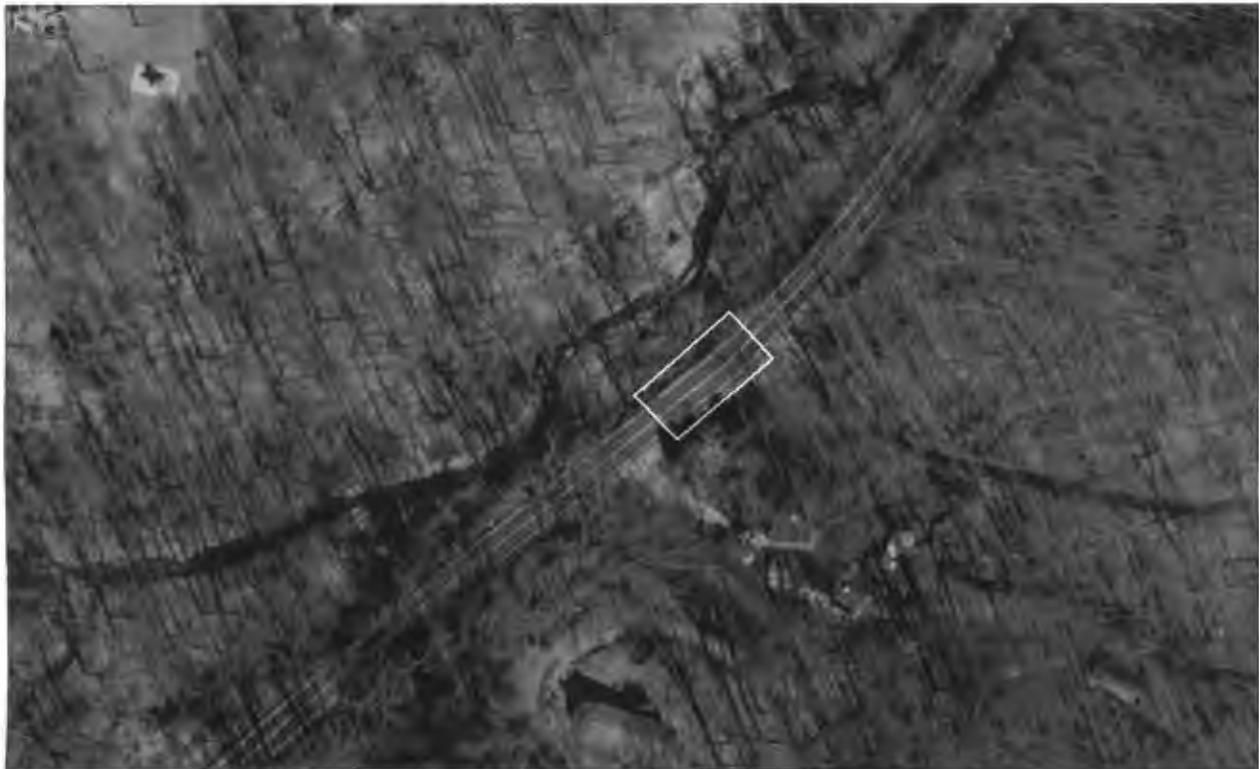
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HO-655  
Bridge (SHA HO-110)  
Henryton Road over tributary of Patapsco River, Marriottsville  
Sykesville Quadrangle 1953, Photorevised 1979



National Web Map Service 6" Orthophoto Map, c. 2011





Inventory # H0-655

Name H0110-HENRYTON RD OVER STREAM

County/State HOWARD / MD

Name of Photographer DAVID DIEHL

Date 2/95

Location of Negative SHA

Description <sup>SOUTH</sup> WEST APPROACH <sup>NORTH</sup> LOOKING EAST

Number <sup>1</sup> ~~8~~ of <sup>4</sup> ~~83~~

NO. 10-4 251801018 \*01



Inventory # 110-655

Name 10110-HENRYTON RD OVER STREAM

County/State HOWARD / MD

Name of Photographer DAVID DIEHL

Date 2/95

Location of Negative SHA

Description EAST  
SOUTH ELEVATION LOOKING

NORTHWEST

Number 2 of 4  
9 of 33

PHOTOGRAPHIC UNIT



Inventory # H0-655

Name H0110-HENRYTON RD OVER STREAM

County/State HOWARD / MD

Name of Photographer DAVID DIEHL

Date 2/95

Location of Negative SHA

Description WEST  
NORTH ELEVATION LOOKING

SOUTHWEST

Number 3 of 4

1-175-205107480\*00



Inventory # H0-655

Name H0110 - HENRYTON RD OVER STREAM

County/State HOWARD / MD

Name of Photographer DAVID DIEHL

Date 2/95

Location of Negative SHA

Description NORTH  
EAST APPROACH LOOKING

WEST

SOUTH

Number 4 of 33

NO PERMISSION